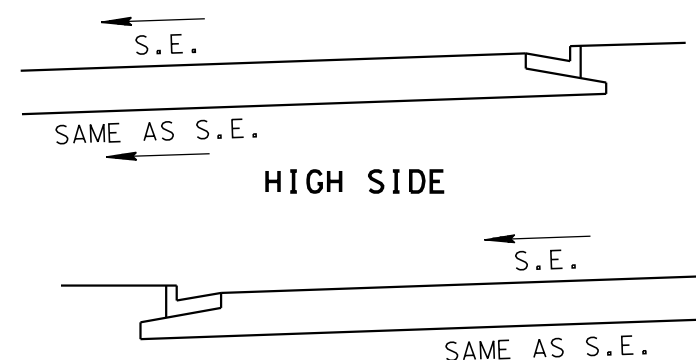
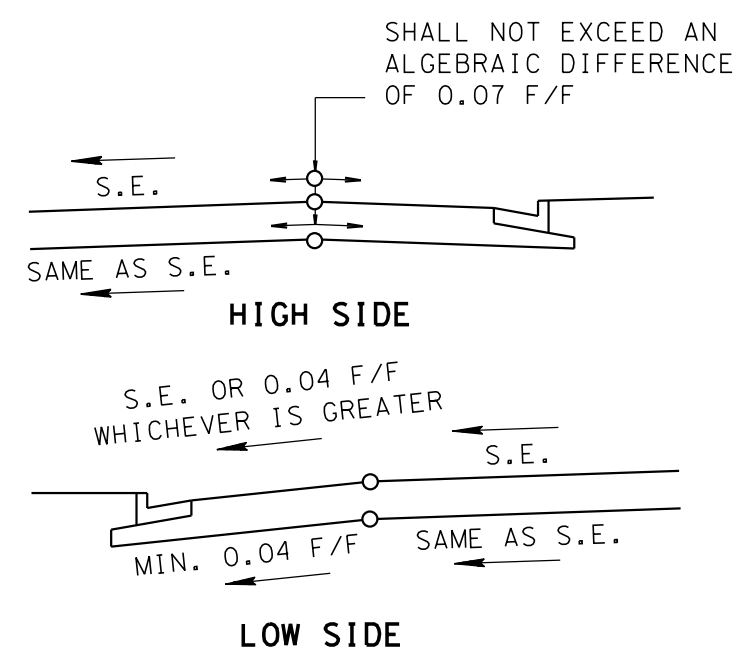


BIKE LANE SHOULDER CROSS SLOPES

ON PROJECTS, WHERE IN THE NEAR FUTURE THE SHOULDER MAY BE USED AS A THROUGH TRAFFIC LANE, THE PAVEMENT SLOPE (0.02 FOOT/FOOT) SHOULD BE USED ON THE SHOULDER IN PLACE OF THE 0.04 FOOT/FOOT SLOPE SHOWN ON THE TYPICAL SECTION. SEE DETAIL FOR SUPERELEVATION OF SHOULDER WITH 0.02 FOOT/FOOT SLOPE.



SUPERELEVATION DETAIL
FOR SHOULDERS



SUPERELEVATION DETAIL
FOR SHOULDERS

GENERAL NOTES

DESIGN SPEED

THESE SECTIONS ARE FOR 45 MILES PER HOUR OR LESS. IF HIGHER SPEED IS PROPOSED USE RD01-TS-6B.

ALIGNMENT

SEE S-PL-6.

SUPERELEVATION AND MEDIAN BARRIERS

SEE APPROPRIATE STANDARD DRAWING IN THE RD01-SE-SERIES AND S-SSMB-SERIES.

SHOULDER

- ① IF SHOULDERS LESS THAN 8', USE RD01-TS-6A.
8' MIN. SHOULDER IS REQUIRED FOR VEHICLE EMERGENCIES AND TO PROVIDE PROPER HORIZONTAL OFFSET TO PEDESTRIAN FACILITIES. UNDER NO CIRCUMSTANCES SHALL THE SHOULDER BE CONSIDERED TO FACILITATE PEDESTRIAN ACCESS. LOCATIONS WHERE EXISTING ROADWAY IS LACKING MIN 8' SHOULDER WIDTH (3R PROJECTS-RESURFACING, RETROFITTING, AND RECONSTRUCTION), MIN 4' SHALL BE PROVIDED. IN SOME INSTANCES, ADJUSTMENT TO LANE WIDTH MAY BE REQUIRED.

CONSTRUCTION EASEMENT

- ② 10' MINIMUM DESIRABLE.

SLOPES

- ③ ON URBAN PROJECTS, THE BACKSLOPE AND FORESLOPE DESIGN WILL VARY FROM PROJECT TO PROJECT; AS A GENERAL RULE USE THE FOLLOWING:
3:1 SLOPES OR FLATTER ARE DESIRABLE AND ARE THE MAXIMUM IN REGION IV. 2:1 SLOPES ARE APPLICABLE IN AREAS WHERE RIGHT-OF-WAY RESTRICTIONS OR COST WARRANTS A STEEPER THAN 3:1 SLOPE.

CURBS

- ④ MEDIAN CURBS WILL BE 6" SLOPING CURBS. OUTSIDE CURBS WILL BE 4" SLOPING CURBS (SEE RP-MC-1). 6" VERTICAL CURBS MAY BE USED IN URBAN ZONES.

SIDEWALKS AND BIKE LANES

SIDEWALK WIDTH SHALL NOT INCLUDE THE 6" WIDTH OF PROPOSED CURB. SIDEWALK SHALL BE A MINIMUM OF 5' WIDE. REFER TO RP-H-SERIES FOR CURB RAMP DETAILS. IF BIKE LANE IS PROPOSED, BIKE LANE SHALL BE PLACED AS FAR AWAY AS POSSIBLE FROM THE EDGE OF TRAVELED WAY. SEE BIKE LANE BUFFER DETAILS ON T-M-12.

PARKING LANES

FOR SPECIFIC CONDITIONS NOT COVERED ON THIS SHEET, REFERENCE SHOULD BE MADE TO "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS," AASHTO, 2011 PAGE NUMBERS 4-72 THROUGH 4-74, 5-14, 6-14, 7-34, AND TO THE CURRENT MUTCD.

3R PROJECTS-RESURFACING, RETROFITTING, AND RECONSTRUCTION

LOCATIONS WHERE EXISTING ROADWAY IS LACKING MIN 8' SHOULDER WIDTH, MIN 4' SHALL BE PROVIDED. IN SOME INSTANCES, ADJUSTMENT TO LANE WIDTH MAY BE REQUIRED. IF MIN 4' SHOULDER CANNOT BE ACHIEVED DUE TO THE ROW LIMITATIONS, UTILITY CONFLICTS, ETC., THEN ADDITIONAL MITIGATION STRATEGIES SUCH AS REDUCING DESIGN SPEED OR GROUND MOUNTED ADVANCED WARNING SIGNS SHALL BE CONSIDERED.

MINOR REVISION -- FHWA
APPROVAL NOT REQUIRED.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
CURB AND GUTTER
SECTIONS
WITH SHOULDER

10-15-02 RD01-TS-6